

From: Matthew Hawkins
Sent: Friday, March 31, 2017 2:22 PM
To: 'lbarlow@spokanevalley.org'
Cc: 'mayor_councilmembers@spokanevalley.org'; 'Frank Cobb'; 'cwiltse@zagmail.gonzaga.edu'
Subject: Painted Hills Golf Course, Redevelopment, Traffic Study Hearing and more

Lori,

I attended the hearing on Wednesday night and found it to be informative and yet lacking in details as to the Traffic study and the safety impact on the adjoining roads and community. While I brought up some of the issues at the hearing I felt that the issues were being brushed aside by the lead engineer and not being addressed.

The primary concerns are as follows:

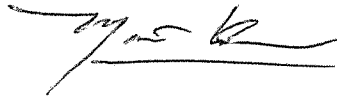
1. Access and safety issues dealing with traffic impacts along Dishman Mica for both the general access from the development as well as Thorpe road have not been addressed. A simple turn lane off of Dishman Mica is not enough when you consider the curvature of Dishman Mica, the elevation changes between the site and roads and the roads themselves.
2. While the pedestrian impacts along 32nd and Pines were addressed at the meeting, the areas along Madison and Thorpe seem to be avoided when you consider the pedestrians along Madison & Thorpe Roads. At all hours of the day you have children and adults both walking and running along these routes. Both the High School and Middle Schools use these routes regularly throughout the year for exercise and conditioning.
3. The impact of the Cyclists along Mohawk and Thorpe have not been addressed. This is an issue on these roads more so than a typical road when you consider that these roads are traditional routes for Cyclists that are out for pleasure rides, club rides and exercise routes. This has never been addressed.
4. The impact of the children walking or riding their bikes to and from school have not been addressed.
5. The load of new residences would seem to indicate that over 1,200 cars would be added to this area, not to mention the added traffic from the Commercial businesses located onsite.

Again, we would have been pleased if we could have addressed these on Wednesday night, but the engineer in charge was focused on closing the meeting with only 10 minutes of input from those in attendance. He also seemed to indicate that it really did not matter what anyone said since he had a letter from the Valley City stating that he had his approvals. If this last point is true, then why the need for a meeting.

Again, I would ask that the above issues be addressed with the needs of the people being recognized. I know that these issues are real since I have children at all three of the schools in question, I live at the bottom of Mohawk Dr and see the runners, walkers, pedestrians, cyclists, children and adults use these routes daily. Cars and Pedestrians have a hard time mixing if left in an unsafe condition. Contrary to the engineers input, we have many conditions in this area that are a pain and unsafe due to the increased traffic even before a redevelopment occurs.

Respectfully

Matt Hawkins
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CITY OF SPOKANE VALLEY